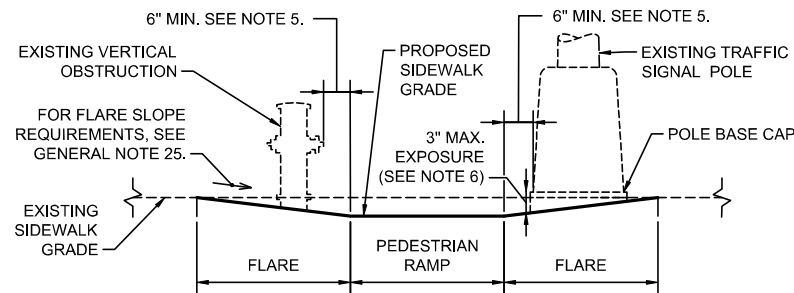
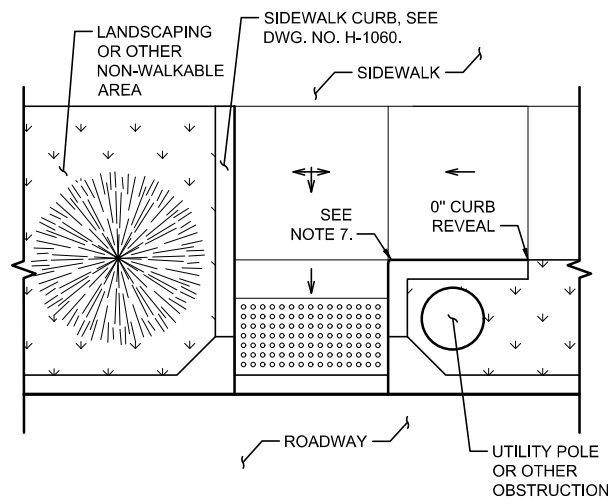


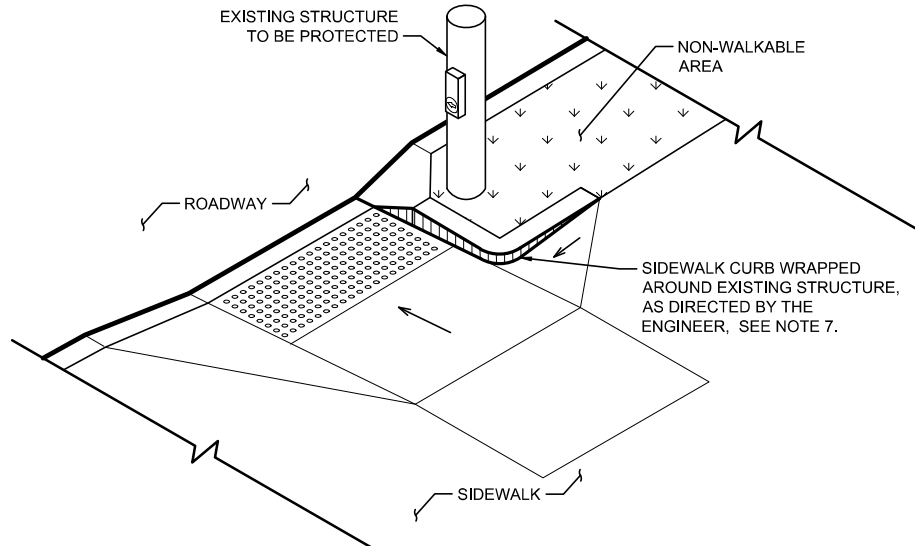
R MEETING NON-COMPLIANT SLOPES - ISOMETRIC VIEW
SCALE: N.T.S.



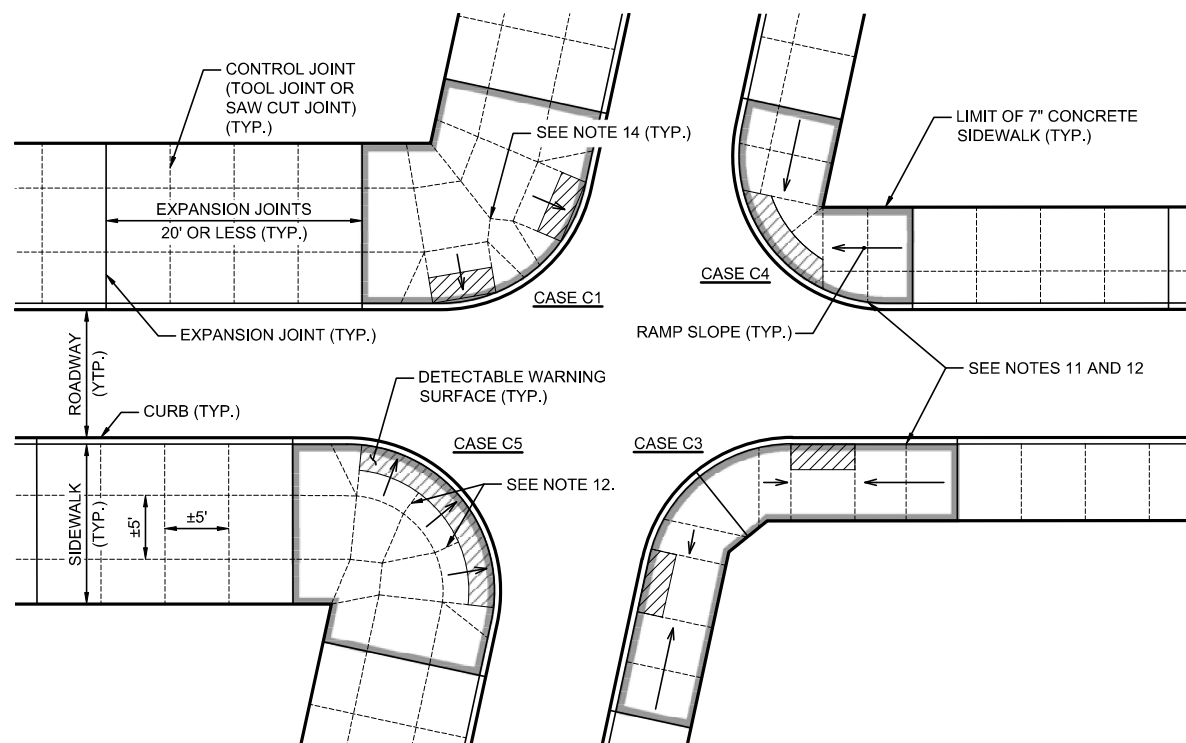
T OBJECTS IN FLARE - ELEVATION VIEW
SCALE: N.T.S.



U EXAMPLE SIDEWALK CURBS AT RAMP - PLAN VIEW
SCALE: N.T.S.



S EXAMPLE SIDEWALK CURB WRAPPED AROUND EXISTING STRUCTURE AT RAMP - ISOMETRIC VIEW
SCALE: N.T.S.



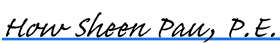


V EXAMPLE CONCRETE SIDEWALK SCORING PATTERN - PLAN VIEW
SCALE: N.T.S.

NOTES:

- FOR INDEX OF DRAWINGS, SLOPE LIMITS, LEGEND, GLOSSARY, GENERAL NOTES, SEE DWG. NO. H-1011-1.
- FOR DETECTABLE WARNING SURFACES (D.W.S.), SEE DWG. NO. H-1011-9.
PEDESTRIAN RAMP SIDE TREATMENT NOTES:
- FOR SIDEWALK CURBS, SEE DWG. NO. H-1060.
- WHEN SITE CONSTRAINTS REQUIRE PEDESTRIAN RAMP TO BE LOCATED IN CLOSE PROXIMITY TO EXISTING STREET FURNITURE SUCH AS UTILITY POLES, STREET LIGHT AND TRAFFIC SIGNAL POLES, HYDRANTS, BOLLARDS, ETC., REFER TO THE DETAILS INCLUDED IN THIS DRAWING, AS DIRECTED BY THE ENGINEER.
- EXISTING VERTICAL OBSTRUCTIONS, EXCLUDING CURBS, SHOULD HAVE A MINIMUM HORIZONTAL CLEARANCE OF SIX INCHES (6") FROM EDGE OF RAMP MEASURED HORIZONTAL TO THE NEAREST PROTRUDING PART BETWEEN A HEIGHT OF SEVEN INCHES (7") AND 80 INCHES. ANY EXCEPTIONS TO THIS NOTE MUST BE APPROVED BY NYC DOT.
- EXISTING NON-HISTORIC AND NON-DECORATIVE TRAFFIC SIGNAL AND STREET LIGHT POLE FOUNDATIONS MUST NOT BE EXPOSED MORE THAN THREE INCHES (3") . USE SIDEWALK CURB IF A MAXIMUM THREE INCH FOUNDATION EXPOSURE IS NOT FEASIBLE.
- WHERE SIDEWALK CURB IS USED TO AVOID A STRUCTURE OR NON-WALKABLE AREA ADJACENT TO A PEDESTRIAN CIRCULATION PATH, SIDEWALK CURB MAY BE WRAPPED AROUND STRUCTURE OR NON-WALKABLE AREA TO ACCOMMODATE REDUCED SIDEWALK GRADE.
CORNER SIDEWALK SCORING GUIDANCE NOTES:
- FOR AN EXAMPLE OF CONCRETE SIDEWALK SCORING OF CONTROL JOINTS AT CORNERS, SEE DETAIL V.
- SCORE ALL SIDEWALK GRADE BREAKS, INCLUDING GRADE BREAKS AT BOTTOM AND TOP OF RAMP SLOPES, GRADE BREAKS OF LANDINGS, AND GRADE BREAKS OF RAMP FLARES.
- SIDEWALK FLAGS SHOULD HAVE A CONSTANT SLOPE; WARPING OF SIDEWALK FLAGS MUST NOT EXCEED THE LIMITS INDICATED ON DETAIL R, THIS SHEET.
- PARALLEL RAMP SHOULD NOT HAVE FLARES SCORED.
- OVERSIZED RAMP AND LANDING FLAGS SHOULD BE SCORED INTO SMALLER FLAGS TO AVOID FLAGS LARGER THAN FIVE FEET (±5').
- SIDEWALK FLAGS SHOULD BE SCORED WITH A NOMINAL WIDTH TO LENGTH RATIO OF ONE TO TWO (1:2) MINIMUM.
- WHERE THREE SCORED SIDEWALK FLAGS MEET, THE MAXIMUM SCORED INTERNAL ANGLE SHOULD NOT EXCEED 150 DEGREES (150°) OF ANY ONE FLAG.

REVISION NO.	DESCRIPTION	DATE	APPROVED

		New York City Department of Transportation	
PEDESTRIAN RAMPS MISCELLANEOUS DETAILS AND EXAMPLES			
Approved:  <u>Roger K. Weld, P.E. (May 24, 2022 13:59 EDT)</u> Chief Engineer Department of Transportation		Approved:  <u>How Sheen Pau, P.E. (May 24, 2022 16:58 EDT)</u> Assistant Commissioner Infrastructure/Design Department of Design + Construction	
Date Issued: 6/06/2022		Scale: AS SHOWN	Drawing #: H-1011-8