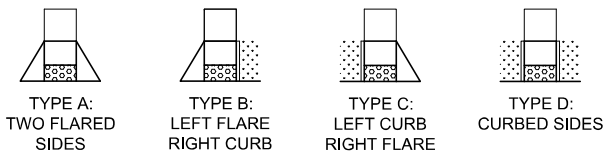
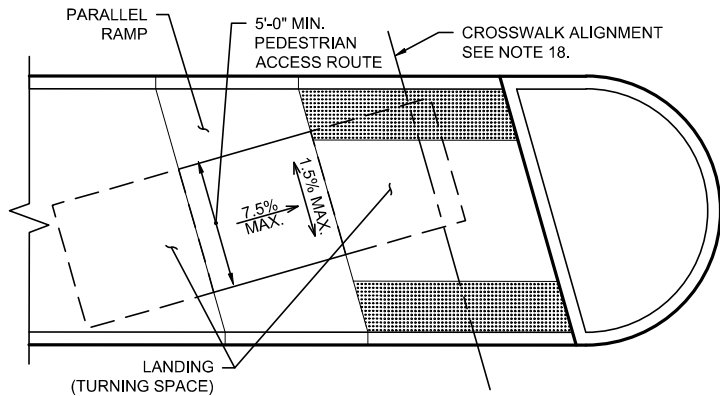


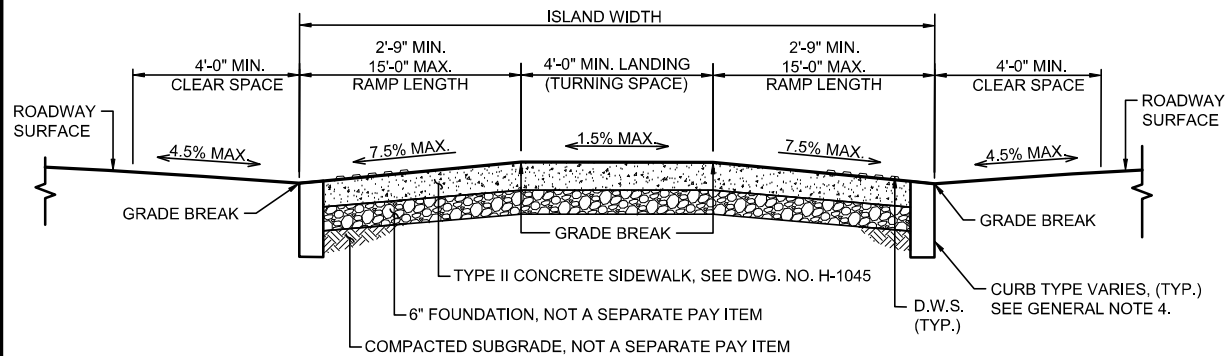
IM1 CASE IM1 - ISLAND WITH PERPENDICULAR RAMPS - ISOMETRIC VIEW
SCALE: N.T.S.



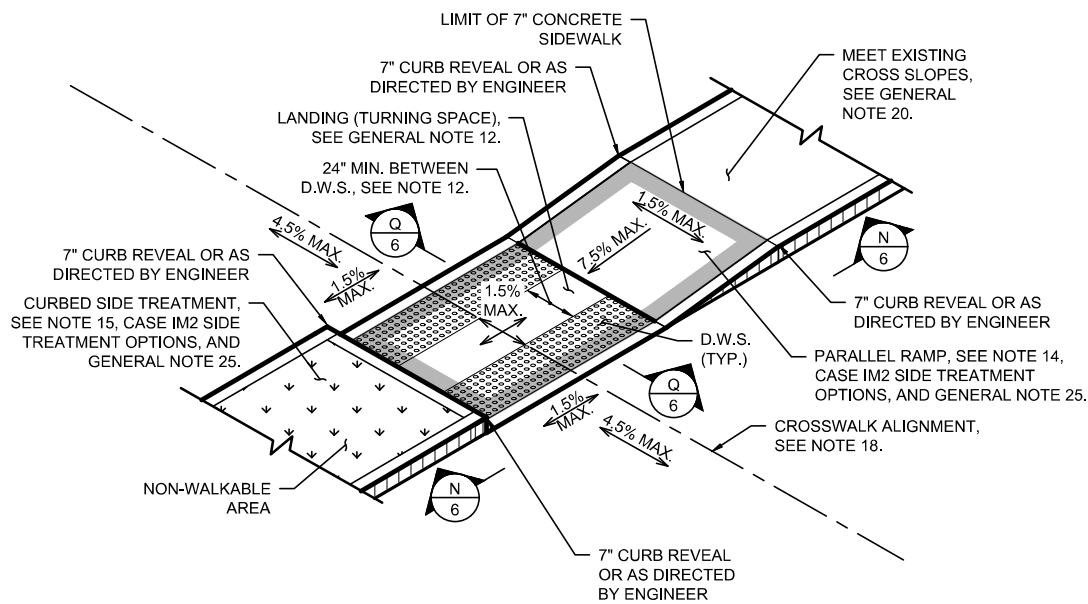
CASE IM1 SIDE TREATMENT OPTIONS
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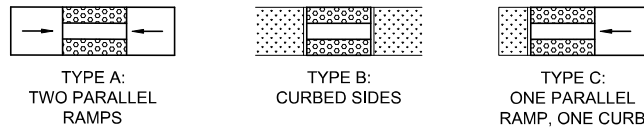
M CASE IM2 AT SKEWED CROSSWALKS DETAIL - PLAN VIEW
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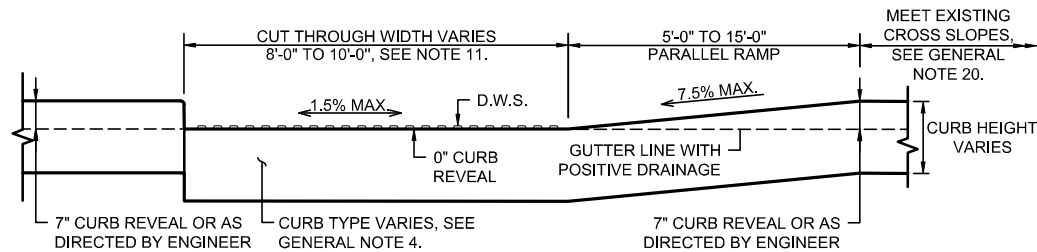
P SECTION VIEW
SCALE: N.T.S.



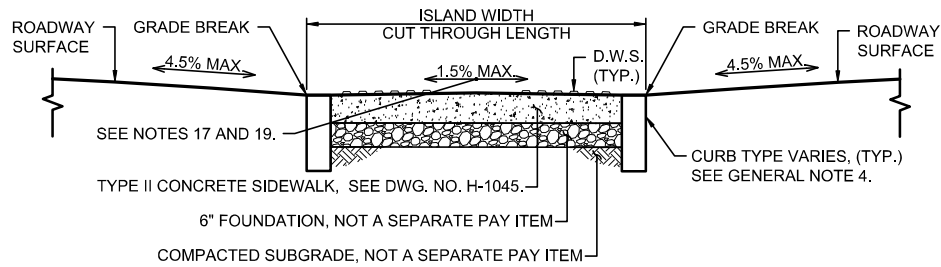
IM2 CASE IM2 - ISLAND CUT THROUGH - ISOMETRIC VIEW
SCALE: N.T.S.



CASE IM2 SIDE TREATMENT OPTIONS
SCALE: N.T.S.



N ELEVATION VIEW
SCALE: N.T.S.



Q SECTION VIEW
SCALE: N.T.S.

NOTES:

- FOR INDEX OF DRAWINGS, SLOPE LIMITS, LEGEND, GLOSSARY, GENERAL NOTES, SEE DWG. NO. H-1011-1.
 - FOR DETECTABLE WARNING SURFACES (D.W.S.), SEE DWG. NO. H-1011-9.
 - FOR OBJECTS IN THE FLARE, EXAMPLE SCORING PATTERNS, EXAMPLE SIDEWALK CURBS AT RAMPS, AND MEETING NON-COMPLIANT SLOPES SEE DWG. NO. H-1011-8.
 - THE GRADING AND GEOMETRY REQUIREMENTS FOR CASE IM1 AND CASE IM2 SUPERSEDE THAT SHOWN IN DWG. NO. H-1003A AND DWG. NO. H-1003B.
 - FOR BUS BOARDING ISLANDS, SEE NYC DOT TYPICAL DESIGN DRAWING NO. BB1-01.
 - FOR TYPICAL PLANTED PEDESTRIAN ISLAND DETAILS, SEE NYC DOT TYPICAL DESIGN DWG. NO. TRF-02.
- CASE IM1 - ISLAND WITH PERPENDICULAR RAMPS NOTES:**
- CASE IM1 MUST BE EVALUATED FOR CONSTRUCTION PRIOR TO SELECTING CASE IM2.
 - CASE IM1 MAY BE USED ON ISLANDS WHERE ISLAND WIDTH IS 16 FEET OR GREATER. WHERE APPROVED BY NYC DOT, CASE IM1 MAY BE USED AT ISLANDS LESS THAN 16 FEET WIDE.
 - ON ISLANDS WITH VARYING WIDTHS AND OTHER SPACE CONSTRAINTS, RAMP ALIGNMENTS MAY BE OFFSET, AS LONG AS TURNING SPACES AND CLEAR SPACES MEET THE REQUIREMENTS OF GENERAL NOTES 12, 13, AND 14.
- CASE IM2 - ISLAND CUT-THROUGH NOTES:**
- CASE IM2 MAY BE USED ON ISLANDS WHERE PROPOSED PEDESTRIAN ISLAND WIDTH IS LESS THAN 16 FEET, OR AS DIRECTED BY THE ENGINEER.
 - CUT THROUGH WIDTHS SHOULD BE CONSTRUCTED IN ACCORDANCE WITH TABLE 3 BELOW. WHERE THERE ARE SITE CONSTRAINTS, THE WIDTH OF A CUT THROUGH MAY BE REDUCED AND MUST NOT BE LESS THAN SIX FEET (6'), WITH NYC DOT APPROVAL, AS DIRECTED BY THE ENGINEER.
 - A MINIMUM SEPARATION OF 24 INCHES MUST BE MAINTAINED BETWEEN D.W.S. IF 24 INCH SEPARATION BETWEEN D.W.S. CANNOT BE ACHIEVED, D.W.S. MUST BE OMITTED.
 - IF D.W.S. IS OMITTED FROM THE CUT THROUGH ON CASE IM2, SIDE TREATMENT OPTIONS TYPE A OR TYPE C, THEN D.W.S. MUST BE PROVIDED ALONG THE FULL WIDTH OF THE SIDE PARALLEL RAMP(S), ABOVE THE BOTTOM GRADE BREAK(S), WITH NYC DOT APPROVAL, AS DIRECTED BY THE ENGINEER.
 - PARALLEL RAMP SIDE TREATMENT OPTIONS MUST BE USED ON ANY SIDEWALK APPROACH TO THE CUT THROUGH THAT CONTAINS A FULL WIDTH PEDESTRIAN CIRCULATION PATH AND IS INTENDED TO PROVIDE PEDESTRIAN ACCESS, AS DIRECTED BY THE ENGINEER.
 - CURBED SIDE TREATMENT OPTIONS MAY BE USED ON ANY SIDEWALK APPROACH TO THE CUT THROUGH THAT CONTAINS A FULL WIDTH NON-WALKABLE AREA NOT INTENDED TO PROVIDE PEDESTRIAN ACCESS, AS DIRECTED BY THE ENGINEER.
 - WHERE CUT THROUGH ADJOIN A PARTIAL WIDTH PEDESTRIAN CIRCULATION PATH INTENDED TO PROVIDE PEDESTRIAN ACCESS, A CASE M1 RAMP MAY BE INSTALLED, AS DIRECTED BY THE ENGINEER.
 - IF THE ELEVATIONS OF BOTH ROADWAY CURBS ON CASE IM2 ARE APPROXIMATELY THE SAME, THE CUT THROUGH MUST BE CONSTRUCTED CROWNED AT THE CENTER, PITCHED TOWARDS BOTH ROADWAY CURBS AT A MINIMUM SLOPE OF ONE HALF PERCENT (0.5%). AS DIRECTED BY THE ENGINEER. IF THE ELEVATIONS DIFFER BETWEEN THE ROADWAY CURBS ON CASE IM2, THE CUT THROUGH MUST BE CONSTRUCTED WITH A CONSTANT SLOPE PERPENDICULAR TO THE ROADWAY CURB, AT A MINIMUM SLOPE OF ONE HALF PERCENT (0.5%).
 - ALL CUT THROUGHS MUST BE PARALLEL TO THE DIRECTION OF THE CROSSWALK. FOR SIDE TREATMENT OPTIONS TYPE A AND TYPE C, A PEDESTRIAN ACCESS ROUTE MUST BE PROVIDED WITH TURNING SPACES. SEE CASE IM2 AT SKEWED CROSSWALKS DETAIL - PLAN VIEW FOR GUIDANCE.
 - WHERE TURNING IS NOT REQUIRED IN A CUT THROUGH, THE SLOPE IN THE DIRECTION OF PEDESTRIAN TRAVEL, PARALLEL TO THE DIRECTION OF THE CROSSWALK, MAY BE INCREASED TO 4.5% MAXIMUM, AS DIRECTED BY THE ENGINEER.

TABLE 3: CUT THROUGH WIDTHS

CROSSWALK WIDTH	LESS THAN 14 FEET	14 FEET OR GREATER
CUT THROUGH WIDTH	8 FEET	10 FEET

CHECKED BY: ee
HWS-H1011

REVISION NO.	DESCRIPTION	DATE	APPROVED

NEW YORK CITY Department of Transportation	
PEDESTRIAN RAMPS ISLAND CASES	
Approved: <u>Roger K. Weld, P.E.</u> Roger K. Weld, P.E. (May 24, 2022 13:59 EDT) Chief Engineer Department of Transportation	Approved: <u>How Sheen Pau, P.E.</u> How Sheen Pau, P.E. (May 24, 2022 16:58 EDT) Assistant Commissioner Infrastructure/Design Department of Design + Construction
Date Issued: 6/06/2022	Scale: AS SHOWN Drawing #: H-1011-6