

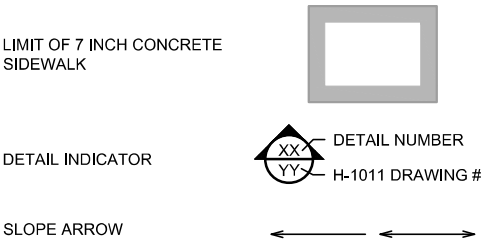
H-1011 INDEX OF DRAWINGS

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TABLE 1: DESIGN, LAYOUT AND WORK ACCEPTANCE SLOPE LIMITS

ELEMENTS	SLOPE LIMITS FOR DESIGN AND FIELD LAYOUT	SLOPE LIMITS FOR WORK ACCEPTANCE
● PEDESTRIAN ACCESS ROUTE CROSS SLOPE ● RAMP CROSS SLOPE ● LANDING (TURNING SPACE) RUNNING SLOPE AND CROSS SLOPE ● ROADWAY GUTTER FLOW SLOPE (SEE GENERAL NOTE 26)	0.5% (1:200) MIN. (SEE GENERAL NOTE 22.) 1.5% (1:67) MAX.	2.0% (1:50) MAX.
● PEDESTRIAN ACCESS ROUTE RUNNING SLOPE (SEE GENERAL NOTE 23) ● BLENDED TRANSITION RUNNING SLOPE ● ROADWAY COUNTER SLOPE	0.5% (1:200) MIN. (SEE GENERAL NOTE 22.) 4.5% (1:22) MAX.	5.0% (1:20) MAX.
● PEDESTRIAN RAMP RUNNING SLOPE	5.0% (1:20) MIN. 7.5% (1:13.5) MAX.	8.3% (1:12) MAX.
● SIDE FLARE INSIDE PEDESTRIAN CIRCULATION PATH (SEE GENERAL NOTE 25)	5.0% (1:20) MIN. 9.5% (1:10.5) MAX.	10.0% (1:10) MAX.
● SIDE FLARE OUTSIDE PEDESTRIAN CIRCULATION PATH (SEE GENERAL NOTE 25)	5.0% (1:20) MIN. 25% (1:4) MAX.	

LEGEND



GLOSSARY

TERMS	DEFINITIONS
CLEAR SPACE	THE TERM "CLEAR SPACE" REFERS TO AN UNOBSTRUCTED AREA PROVIDED BEYOND THE BOTTOM GRADE BREAK OF A RAMP, FOUR FEET BY FOUR FEET (4' X 4') SQUARE MINIMUM, WITHIN THE WIDTH OF THE MARKED OR UNMARKED CROSSWALK, AND WHOLLY OUTSIDE OF ANY VEHICULAR TRAVEL LANE (NON-PARKING LANE) RUNNING PARALLEL TO THE CROSSWALK. THE CLEAR SPACE MAY OVERLAP BOTTOM LANDING AREAS, AND FLUSH CURBS. IF THE CLEAR SPACE IS ALSO A TURNING SPACE, THEN THE SLOPES OF THE CLEAR SPACE MUST COMPLY WITH THE REQUIREMENTS FOR TURNING SPACE.
COUNTER SLOPE	THE TERM "COUNTER SLOPE" REFERS TO THE SLOPE OF THE ROADWAY AT THE FLUSH CURB, PERPENDICULAR TO THE CURB LINE.
CROSS SLOPE	THE TERM "CROSS SLOPE" REFERS TO ANY SLOPE MEASURED PERPENDICULAR TO THE PREDOMINANT DIRECTION OF PEDESTRIAN TRAVEL. CROSS SLOPE MAY ALSO BE REFERRED TO AS TRANSVERSE SLOPE.
CROSSWALK	THE TERM "CROSSWALK" REFERS TO BOTH MARKED AND UNMARKED CROSSWALKS. REFER TO THE NYC TRAFFIC RULES, AND NYC DOT PAVEMENT MARKINGS STANDARDS.
GUTTER FLOW SLOPE	THE TERM "GUTTER FLOW SLOPE" REFERS TO THE ROADWAY SLOPE AT THE FLUSH CURB, PARALLEL TO THE CURB LINE.
LANDING	THE TERM "LANDING " REFERS TO AN UNOBSTRUCTED LEVEL AREA ADJOINING A RAMP THAT ALLOWS A PEDESTRIAN USING A WHEELCHAIR TO REST WHEN USING THE RAMP. A LANDING HAS CROSS SLOPES AND RUNNING SLOPES NO GREATER THAN TWO PERCENT (2.0%). A PERPENDICULAR RAMP TYPICALLY HAS A LANDING AT THE TOP OF THE RAMP RUN. A PARALLEL RAMP TYPICALLY HAS A LANDING AT THE BOTTOM OF THE RAMP RUN.
NON-WALKABLE AREA	THE TERM "NON-WALKABLE AREA" REFERS TO AN AREA EXCLUSIVE OF ANY PEDESTRIAN ACCESS ROUTE AND PEDESTRIAN CIRCULATION PATH WHICH IS NOT INTENDED FOR PEDESTRIAN TRAVEL. SOME EXAMPLES ARE PLANTING STRIPS, TREE PITS, AND ROUGH HEWN (NON-ADA COMPLIANT) GRANITE BLOCK PAVERS.
PARALLEL RAMP	THE TERM "PARALLEL RAMP" REFERS TO A PEDESTRIAN RAMP THAT HAS A RUNNING SLOPE PARALLEL TO THE CURB LINE.
PEDESTRIAN ACCESS ROUTE	THE TERM "PEDESTRIAN ACCESS ROUTE" REFERS TO A CONTINUOUS AND UNOBSTRUCTED PATH OF TRAVEL PROVIDED FOR PEDESTRIANS WITH DISABILITIES WITHIN OR COINCIDING WITH A PEDESTRIAN CIRCULATION PATH. THE PEDESTRIAN ACCESS ROUTE IS INCLUSIVE OF WALKING SURFACES WITH RUNNING SLOPES NOT STEEPER THAN FIVE PERCENT (5.0%), DOORWAYS, AND PEDESTRIAN RAMPS. ADDITIONALLY, THE PEDESTRIAN ACCESS ROUTE IS INCLUSIVE OF LANDINGS, TURNING SPACES, BLENDED TRANSITIONS, CUT THROUGHS, AND MAY INCLUDE FLUSH CASTINGS AND HARDWARE. THE PEDESTRIAN ACCESS ROUTE DOES NOT INCLUDE THE FLARED SIDES OF PEDESTRIAN RAMPS. THE CROSS SLOPE OF A PEDESTRIAN ACCESS ROUTE MUST NOT BE GREATER THAN TWO PERCENT (2.0%).
PEDESTRIAN CIRCULATION PATH	THE TERM "PEDESTRIAN CIRCULATION PATH" REFERS TO A PREPARED SURFACE PROVIDED FOR PEDESTRIAN TRAVEL IN THE PUBLIC RIGHT-OF-WAY. THIS MAY CONSIST OF CONCRETE SIDEWALK, ADA COMPLIANT GRANITE BLOCK PAVEMENT, BLUESTONE FLAGS, ASPHALT BLOCK PAVERS, AND BRICK PAVERS, NYC DOT STANDARD HIGHWAY SPECIFICATION SECTIONS 4.13, 6.04 ADA, 6.07, 6.60, 6.66, OR OTHER PREPARED WALKING SURFACES. FOR EXAMPLE, THE PEDESTRIAN CIRCULATION PATH DOES NOT INCLUDE TREE PITS, PLANTING STRIPS, OR OTHER NON-WALKABLE AREAS. THE PEDESTRIAN CIRCULATION PATH INCLUDES THE FLARED SIDES OF PEDESTRIAN RAMPS.
PERPENDICULAR RAMP	THE TERM "PERPENDICULAR RAMP" REFERS TO A PEDESTRIAN RAMP THAT HAS A RUNNING SLOPE THAT IS APPROXIMATELY PERPENDICULAR TO THE CURB LINE.
RUNNING SLOPE	THE TERM "RUNNING SLOPE" REFERS TO ANY SLOPE MEASURED PARALLEL TO THE PREDOMINANT DIRECTION OF PEDESTRIAN TRAVEL. RUNNING SLOPE MAY ALSO BE REFERRED TO AS LONGITUDINAL SLOPE.
SIDE FLARE	THE TERM "SIDE FLARE" REFERS TO THE FLARED SIDE OF A PERPENDICULAR RAMP, WHOSE SLOPE IS MEASURED PARALLEL TO THE CURB LINE.
SIDEWALK TRANSITION RAMP	THE TERM "SIDEWALK TRANSITION RAMP" REFERS TO A PEDESTRIAN RAMP RUNNING PARALLEL TO THE CURB LINE TRANSITIONING BETWEEN DIFFERENT SIDEWALK GRADES. THE SLOPE OF A SIDEWALK TRANSITION RAMP IS GREATER THAN FIVE PERCENT (5.0%) AND IS NOT GREATER THAN 8.3%.
TURNING SPACE	THE TERM "TURNING SPACE" REFERS TO AN UNOBSTRUCTED LEVEL AREA IN A PEDESTRIAN ACCESS ROUTE THAT PROVIDES SUFFICIENT SPACE TO ALLOWS A PEDESTRIAN USING A WHEELCHAIR TO PERFORM A TURNING MOVEMENT. TURNING SPACES MAY OVERLAP OTHER AREAS WITHIN THE PEDESTRIAN ACCESS ROUTE, SUCH AS CLEAR SPACES AND LANDINGS. THE CROSS SLOPES AND RUNNING SLOPES OF A TURNING SPACE MUST BE NO GREATER THAN TWO PERCENT (2.0%).

GENERAL NOTES:

- THESE DRAWINGS PROVIDE PEDESTRIAN RAMP (CURB RAMP), BLENDED TRANSITION, AND CUT THROUGH DESIGN GUIDANCE IN ACCORDANCE WITH THE 2010 AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN (ADA 2010), AND THE PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG). ALL NEW YORK CITY DEPARTMENT OF TRANSPORTATION (NYC DOT) REQUIREMENTS MUST BE MET WHILE ACCOMMODATING EXISTING CONDITIONS. ANY DEVIATION FROM THESE NYC DOT STANDARDS MUST BE DOCUMENTED AND SUBMITTED TO NYC DOT FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
- PEDESTRIAN RAMPS, BLENDED TRANSITIONS, CUT THROUGHS, MATERIALS, AND CONSTRUCTION METHODS EMPLOYED MUST COMPLY WITH THE LATEST VERSION OF THE NYC DOT HIGHWAY RULES, NYC DOT STANDARD HIGHWAY SPECIFICATIONS, AND ADA 2010.
- THE SURFACE OF ALL PEDESTRIAN ACCESS ROUTES, INCLUDING RAMPS, AND DETECTABLE WARNING SURFACES, MUST BE FIRM, STABLE, AND SLIP RESISTANT. CONCRETE RAMP SURFACES MUST HAVE A COARSE BROOM FINISH RUNNING PERPENDICULAR TO THE RUNNING SLOPE.
- FOR STEEL FACED CURB CONSTRUCTION AT PEDESTRIAN RAMPS, BLENDED TRANSITIONS, AND CUT THROUGHS, SEE DWG. NO. H-1060. FOR SIDEWALK CURB CONSTRUCTION, SEE DWG. NO. H-1060. FOR ROADWAY CURB CONSTRUCTION, SEE DWGS. H-1010, H-1035, H-1036, H-1044, H-1056, H-1056-A, AND H-1060.
- ON CONCRETE SIDEWALKS, EXPANSION JOINTS MUST BE PLACED AT ALL BUILDING FAÇADES AND STRUCTURES. EXPANSION JOINTS, TOOLED, OR SAW-CUT JOINTS (DUMMY JOINTS) MUST COMPLY WITH THE REQUIREMENTS OF NYC DOT STANDARD HIGHWAY SPECIFICATION SECTION 4.13. JOINTS BETWEEN SIDEWALKS, PEDESTRIAN RAMPS, LANDING AREAS, AND ROADWAYS MUST BE FLUSH AND FREE FROM ABRUPT VERTICAL CHANGES.
- DETECTABLE WARNING SURFACES MUST BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS ON DWG. NO. H-1011-9.
- PRIOR TO POURING CONCRETE OR LAYING PAVING, THE CONTRACTOR MUST MAKE AVAILABLE FOR INSPECTION BY THE ENGINEER, FORM WORK, GRADE STAKES, OR ROUGH GRADING, TO DEMONSTRATE THAT LAYOUT GRADES AND DIMENSIONS MATCH DESIGN GRADES AND DIMENSIONS.
- ELEMENTS MUST BE CONSTRUCTED TO MEET THE SLOPE REQUIREMENTS OF TABLE 1 AND THE DIMENSIONAL REQUIREMENTS HEREIN. IF MEETING THESE REQUIREMENTS IS NOT POSSIBLE, A DOCUMENTATION OF TECHNICAL INFEASIBILITY MUST BE APPROVED PRIOR TO CONSTRUCTION, SEE GENERAL NOTE 11.
- PEDESTRIAN ACCESS ROUTE SURFACES MUST BE CONSTRUCTED FLUSH. VERTICAL CHANGES IN LEVEL MUST NOT EXCEED ONE QUARTER INCH (1/4"). HORIZONTAL OPENINGS IN GROUND SURFACES MUST NOT EXCEED ONE HALF INCH (1/2"). ELONGATED OPENINGS, SUCH AS AT DRAINAGE INLETS, MUST BE PLACED PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL.
- EXPANSION JOINTS, TOOLED OR SAW-CUT JOINTS (DUMMY JOINTS), MUST BE CONSTRUCTED AT ALL GRADE BREAKS WITHIN THE PEDESTRIAN ACCESS ROUTE. INSTALL JOINTS WHERE PEDESTRIAN RAMPS, LANDINGS (TURNING SPACES), FLARES, AND SIDEWALKS ABUT.

DESIGN AND FIELD LAYOUT NOTES:

- WHERE SITE CONSTRAINTS PROHIBIT PEDESTRIAN RAMP, BLENDED TRANSITION, AND CUT THROUGH ELEMENTS FROM BEING IN FULL AND STRICT COMPLIANCE WITH THE MINIMUM REQUIREMENTS, PEDESTRIAN RAMP, BLENDED TRANSITION, OR CUT THROUGH ELEMENTS MAY BE DESIGNED AND LAID OUT TO THE MAXIMUM EXTENT FEASIBLE. ANY PEDESTRIAN RAMP, BLENDED TRANSITION, OR CUT THROUGH THAT IS NOT IN FULL AND STRICT COMPLIANCE WITH THE MINIMUM REQUIREMENTS, MUST BE DOCUMENTED BY THE ENGINEER, ARCHITECT, OR LANDSCAPE ARCHITECT, AND APPROVED BY NYC DOT OR NYC DDC AS BEING TECHNICALLY INFEASIBLE.
- LANDING (TURNING SPACE) REQUIREMENTS:
 - A LANDING (TURNING SPACE) MUST BE PROVIDED AT THE TOP OF PERPENDICULAR RAMPS AND BLENDED TRANSITIONS, AND AT THE BOTTOM OF PARALLEL RAMPS.
 - LANDINGS MAY OVERLAP WITH ADJACENT LANDINGS.
 - WHERE MULTIPLE LANDINGS ARE SEPARATED BY A DISTANCE LESS THAN TWO FEET (2'), THEY MUST BE COMBINED INTO A SHARED LANDING.
 - A SINGLE LANDING MAY SERVE MORE THAN ONE RAMP OR BLENDED TRANSITION.
 - A LANDING MUST NOT OVERLAP A RAMP OR BLENDED TRANSITION.
 - A LANDING CONSTRAINED BETWEEN A PEDESTRIAN RAMP OR BLENDED TRANSITION AND AN OBSTACLE MUST HAVE A MINIMUM LENGTH OF FIVE FEET (5') BETWEEN THE RAMP OR BLENDED TRANSITION AND OBSTACLE. A LANDING THAT IS UNCONSTRAINED MUST HAVE A MINIMUM LENGTH OF FOUR FEET (4').
 - A LANDING MUST BE PROVIDED FOR THE FULL WIDTH OF A PEDESTRIAN RAMP OR BLENDED TRANSITION, EXCEPT WHERE INDICATED, BUT NOT LESS THAN FOUR FEET (4').
- WHERE FEASIBLE, THE RUNNING SLOPE OF PERPENDICULAR RAMPS SHOULD ALIGN PARALLEL WITH THE MARKED OR UNMARKED CROSSWALK. IF A RAMP IS NOT ALIGNED WITH THE MARKED OR UNMARKED CROSSWALK, THE CLEAR SPACE MUST BE DESIGNED WITH 1.5% MAXIMUM RUNNING AND CROSS SLOPES.
- AT MARKED CROSSWALKS, PEDESTRIAN RAMPS (EXCLUDING FLARES) AND CUT THROUGHS MUST BE LOCATED WITHIN THE CROSSWALK MARKINGS. BEYOND THE PEDESTRIAN RAMP BOTTOM GRADE BREAK, A FOUR FEET BY FOUR FEET (4' X 4') SQUARE CLEAR SPACE MUST BE PROVIDED WITHIN MARKED OR UNMARKED CROSSWALKS.

- ALL DIMENSIONS AND NOTES WILL BE APPLICABLE TO ANY PEDESTRIAN RAMP CURB INSTALLATION (NYC DOT STANDARD HIGHWAY SPECIFICATION SECTION 4.07, 4.08, OR 4.09), INCLUDING INTEGRAL CURB AND GUTTER.
- GRADE BREAKS AT THE TOP AND BOTTOM OF PEDESTRIAN RAMPS MUST BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN.
- THE WIDTH OF A PEDESTRIAN RAMP SHOULD NOT BE LESS THAN FIVE FEET (5'). WHERE THERE ARE SITE CONSTRAINTS, THE WIDTH OF A PEDESTRIAN RAMP MAY BE LESS THAN FIVE FEET (5') BUT MUST NOT BE LESS THAN FOUR FEET (4'), AS APPROVED BY NYC DOT. PEDESTRIAN RAMPS MAY BE WIDER THAN FIVE FEET (5'), AS DIRECTED BY THE ENGINEER.
- PEDESTRIAN RAMP LENGTH MAY BE LIMITED TO 15 FEET TO AVOID CHASING SIDEWALK GRADES. A 15 FOOT LONG PEDESTRIAN RAMP WITH A SLOPE THAT EXCEEDS 8.3% MAY BE CONSIDERED CONSTRUCTED TO THE MAXIMUM EXTENT FEASIBLE AS APPROVED (SEE NOTE 11.). THE MAXIMUM RISE OF ANY RAMP RUN MUST NOT EXCEED 30 INCHES.
- FOR PERPENDICULAR RAMPS AND BLENDED TRANSITIONS, 5 FEET TO 15 FEET SIDEWALK TRANSITION RAMPS PARALLEL TO THE CURB LINE MAY BE USED TO CONNECT PROPOSED RAMP LANDINGS AND TURNING SPACES WITH EXISTING SIDEWALK APPROACHES, AS DIRECTED BY THE ENGINEER.
- WHERE NEW SIDEWALK OR RAMP CONSTRUCTION ADJOINS EXISTING SIDEWALK WITH CROSS SLOPES THAT EXCEED ADA COMPLIANT DESIGN AND CONSTRUCTION LIMITS, NEW SIDEWALK CROSS SLOPES CAN TRANSITION TO EXISTING SIDEWALK CROSS SLOPES AT A RATE OF CHANGE NO GREATER THAN ONE PERCENT (1.0%) PER LONGITUDINAL FOOT, AS DIRECTED BY THE ENGINEER.
- WHERE PROPOSED ON SHARED USE PATHS, PEDESTRIAN RAMPS, BLENDED TRANSITIONS AND CUT THROUGHS MUST EXTEND THE FULL WIDTH OF THE SHARED USE PATH. WHERE BICYCLE RAMPS WITHIN PEDESTRIAN CIRCULATION PATHS CONTAIN FLUSH CURBS AT STREET CROSSINGS, A DETECTABLE WARNING SURFACE MUST BE INSTALLED.
- POSITIVE DRAINAGE MUST BE PROVIDED. RAMP CONSTRUCTION MUST NOT HINDER POSITIVE DRAINAGE.
- SIDEWALK LONGITUDINAL SLOPES MUST BE DESIGNED AND LAID OUT TO NOT EXCEED 4.5%, EXCEPT AT RAMPS AND RAMP FLARES. WHERE LONGITUDINAL ROADWAY PROFILE EXCEED 4.5%, SIDEWALK LONGITUDINAL SLOPES MUST BE DESIGNED AND LAID OUT TO NOT EXCEED THE ROADWAY SLOPE.
- THE DIFFERENCE IN SLOPE BETWEEN THE BOTTOM OF A PERPENDICULAR RAMP AND THE ADJOINING ROADWAY COUNTER SLOPE (CROSS SLOPE) SURFACE MUST NOT EXCEED 13.3%. A MINIMUM TWO FOOT (2') LONG LEVEL AREA MAY BE DESIGNED AND LAID OUT BETWEEN RAMP SLOPE AND ROADWAY COUNTER SLOPE, AS DIRECTED BY THE ENGINEER.
- WHERE THE SIDE OF A PEDESTRIAN RAMP ADJOINS A PEDESTRIAN CIRCULATION PATH, A SIDE FLARE MUST BE DESIGNED AND LAID OUT WITH A SLOPE NO GREATER THAN 9.5%. WHERE THE SIDE OF A PEDESTRIAN RAMP ADJOINS A NON-WALKABLE AREA, A SIDE FLARE MAY BE DESIGNED AND LAID OUT WITH A SLOPE NO GREATER THAN 25%, OR A SIDEWALK CURB MAY BE DESIGNED AND LAID OUT, AS DIRECTED BY THE ENGINEER. FOR OBJECTS IN THE FLARE, SEE DWG NO. H-1011-8. FOR SIDEWALK CURB STANDARDS, SEE DWG. NO. H-1060. FOR SPECIFIC OPTIONS FOR EACH PEDESTRIAN RAMP CASE, SEE THE SIDE TREATMENT DETAILS FOR EACH CASE.
- AT SIGNAL CONTROLLED OR UNCONTROLLED CROSSINGS WHERE THE EXISTING GUTTER FLOW SLOPE (ROADWAY PROFILE) EXCEED TWO PERCENT (2.0%) AND STREET RE-GRADING IS NOT FEASIBLE, THE SIDEWALK SLOPE AT THE FLUSH CURB (BOTTOM RAMP LANDING OR RAMP CROSS SLOPE) PARALLEL TO THE GUTTER FLOW SLOPE MUST MATCH THE EXISTING GUTTER FLOW SLOPE (ROADWAY PROFILE), AS DOCUMENTED AND APPROVED BY THE ENGINEER.

CASE SELECTION NOTES:

- FOR CORNERS WHERE THE PEDESTRIAN CIRCULATION PATH IS EIGHT FEET (8') WIDE OR GREATER, USE PERPENDICULAR CASE C1 OR C2, DWG. NO. H-1011-2. FOR ADDITIONAL SELECTION CRITERIA FOR CASE C2 SEE NOTE 11. ON DWG. NO. H-1011-2. WHERE ARC LENGTH ALONG CURB RETURN IS LIMITED, USE CASE C5, DWG. NO. H-1011-4, AS DIRECTED BY THE ENGINEER.
- FOR CORNERS WHERE THE PEDESTRIAN CIRCULATION PATH IS LESS THAN EIGHT FEET (8') WIDE, USE PARALLEL CASE C3, DWG. NO. H-1011-3. WHERE ARC LENGTH ALONG CURB RETURN IS LIMITED, USE CASE C4, DWG. NO. H-1011-4, AS DIRECTED BY THE ENGINEER.
- FOR RAMPS NOT LOCATED AT A SIDEWALK CORNER, USE CASE M1 OR M2, DWG. NO. H-1011-5.
- FOR ISLANDS WITHIN PEDESTRIAN CROSSINGS, USE ISLAND CASES: CASE IM1 OR IM2, DWG. NO. H-1011-6.
- FOR TEMPORARY RAMPS, AND RAMP REQUIREMENTS IN CONSTRUCTION WORK ZONES, SEE TEMPORARY CASES: CASE T1, T2, OR T3, DWG. NO. H-1011-7.

<div><div>NEW YORK CITY</div><div>DOT</div></div>		New York City Department of Transportation	
PEDESTRIAN RAMPS INDEX OF DRAWINGS, SLOPE LIMITS, LEGEND, GLOSSARY, GENERAL NOTES			
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